

31.



# City of West Allis Matter Summary

7525 W. Greenfield Ave.  
West Allis, WI 53214

File Number	Title	Status
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2007-0359      Communication      In Committee

Communication from City Engineer on the Plan Commission action and recommended plan for the reconstruction of W. Dixon Street.

Introduced: 6/5/2007      Controlling Body: Public Works Committee

### COMMITTEE RECOMMENDATION

APPROVE &  
PLACE ON FILE

ACTION DATE:	MOVER	SECONDER		AYE	NO	PRESENT	EXCUSED
			Barczak				
			Czaplewski				
6-5-07		✓	Dobrowski	✓			
			Kopplin				✓
			Lajsic				
			Narlock	✓			
			Reinke				
	✓		Sengstock	✓			
			Vitale	✓			
			Weigel				
			TOTAL	4	-		1

### SIGNATURE OF COMMITTEE MEMBER

*Paul Hall*      \_\_\_\_\_      \_\_\_\_\_  
Chair      Vice-Chair      Member

### COMMON COUNCIL ACTION

PLACE ON FILE

ACTION DATE:	MOVER	SECONDER		AYE	NO	PRESENT	EXCUSED
			Barczak	✓			
			Czaplewski	✓			
JUN 05 2007		✓	Dobrowski	✓			
			Kopplin				✓
			Lajsic	✓			
	✓		Narlock	✓			
			Reinke	✓			
			Sengstock	✓			
			Vitale	✓			
			Weigel	✓			
			TOTAL	9	-		1



**ENGINEERING DEPARTMENT**

**Michael G. Lewis**  
City Engineer

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## Memorandum

**TO:** Richard Narlock, Chair, Board of Public Works  
Vincent Vitale, First District  
Kurt Kopplin, Second District  
Linda Dobrowski, Fourth District  
James Sengstock, Fifth District

**FROM:** Michael Lewis, City Engineer

**DATE:** June 1, 2007.

**RE:** **Communication from City Engineer Regarding Proposed Plan for the Reconstruction of Dixon Street from 116<sup>th</sup> to Curtis Road and 118<sup>th</sup> from Fairview Avenue and Dixon Street**

During the review and approval of this reconstruction project, the Board of Public Works requested that they have the opportunity to review and approve the plan. Attached is the staff report to the Plan Commission and a plan showing the proposed layout. The recommendation from the Plan Commission is to approve the plan.

My staff talked with or met with a few of the property owners along the project and tried to incorporate their concerns as much as possible. The main points we wanted in the new street was to "green" it up (it's a sea of asphalt now), stop the illegal parking and use standard driveways to control the traffic. As these businesses remodel or expand, they will have to comply with the City's site and architectural review standards and we didn't want to encourage them to point a finger at an ugly street to try to justify some of their complaints.

**Cc:** Alderman Gary Barczak, Third District

**STAFF REPORT  
WEST ALLIS PLAN COMMISSION  
WEDNESDAY, MAY 23, 2007**

**1. Approval of minutes of the April 25, 2007, meeting.**

Submitted under separate cover.

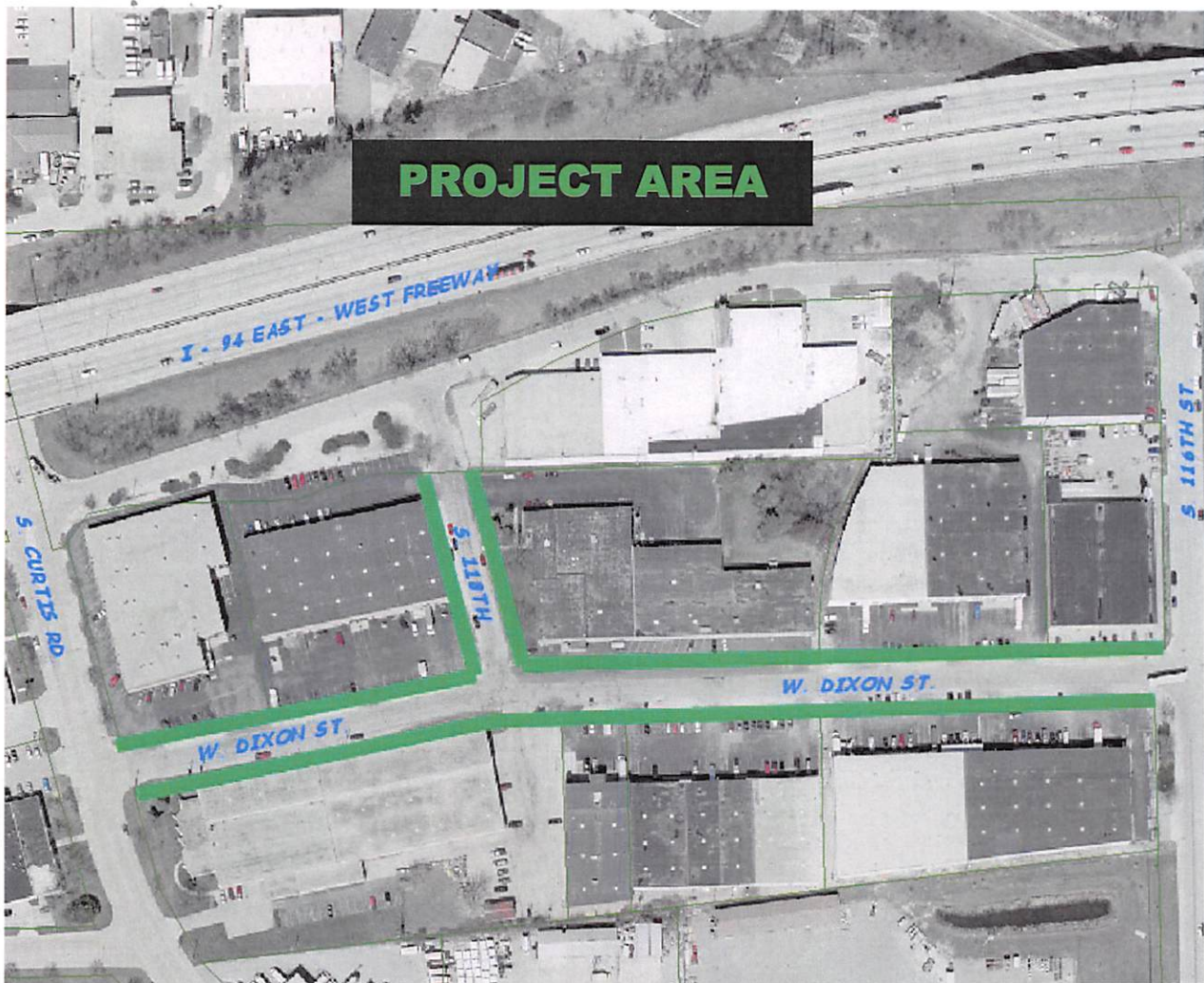
**Recommendation:** Approve as submitted and/or as modified.

**2. Streetscaping Plan for proposed landscaping within City right of way at W. Dixon Street from S. 116 St. to S. Curtis Rd. as well as S. 118 St. from W. Fairview Ave. to W. Dixon St.**

On December 5, 2006, the West Allis City Council approved the Engineering Department's proposal to reconstruct W. Dixon Street from S. 116 St. to S. Curtis Rd. as well as S. 118 Street from W. Fairview Avenue to W. Dixon Street. The existing asphalt pavement was constructed between 1962 and 1968 and is severely deteriorated. As part of this project, the Engineering Department is proposing the addition of landscaping within the City's right of way including the provision of new trees and grass in the terrace areas. The City's Forestry Superintendent has tentatively designed the location of 42 new trees spaced approximately 40 feet apart along Dixon and 118 Streets. These streets are within an area zoned M-1 Manufacturing where there is very little vegetation at all (there are only 7 existing trees in the ROW). The majority of the private properties are also missing any landscaping or vegetation and are mostly paved with asphalt or gravel over the entire lot.



Some of the properties on Dixon Street between S. 116 and 118 Streets have historically experienced a shortage of parking on site and have used the City's right of way as an extension of their private parking lots. The proposed addition of trees and grass in the terrace will eliminate some of these parking areas where vehicles currently park perpendicular to the road. These properties were all formerly issued specific driveway permits by the Board of Public Works between 1962 and 1988. Unfortunately, the lack of curb and gutter has rendered some of these permits irrelevant since vehicles currently enter and exit the roadway from anywhere along these properties. The new concrete pavement will resolve drainage problems and enhance safety with the addition of new concrete curb and gutter. The existing driveway permits will all be honored by the provision of curb openings in the new curb and gutter, in accordance with their originally approved site plans. The new 40 foot wide pavement will also allow for parallel parking on the street which is not available on the current 30 foot wide pavement.



The principal advantages of controlling access (by only allowing specifically permitted driveways) are the upgrading of service and safety. With control of access, permitted entrances and exits are located at points best suited to fit traffic and land-use needs and are designed to enable vehicles to enter and leave safely with a minimum of interference with through traffic. Vehicles are prevented from entering or leaving elsewhere so that a high quality of service is preserved and the accident potential is lessened. On streets where there is no control of access (similar to the current situation on Dixon Street), interference from the roadside access can become a major factor in reducing the capacity, increasing the accident potential, and eroding the mobility function that the facility was designed to provide.

The City has also received permission to remove the railroad spur track crossing on Dixon Street. This spur was formerly connected to the mainline tracks of the Canadian Pacific Railroad which have been removed to allow construction of a new bike trail by the Wisconsin Department of Natural Resources.

**Recommendation:** Recommend approval of the City Engineering Departments streetscaping plan for proposed landscaping within City right of way at W. Dixon Street from S. 116 St. to S. Curtis Rd. as well as S. 118 St. from W. Fairview Ave. to W. Dixon St. in accordance with the City Foresters recommendation.

- 3A. Special Use Permit for proposed BUNKERS' nostalgic lounge & eatery restaurant/bar, to be located at 7420 W. Greenfield Ave. (Tax Key No. 440-0413-000)
- 3B. Site, Landscaping and Screening, and Floor Plans for proposed BUNKERS' nostalgic lounge & eatery restaurant/bar, to be located at 7420 W. Greenfield Ave., submitted by Scott Bunker, owner. (Tax Key No. 440-0413-000)

SOUTH CURTIS ROAD

•2332

WEST DIXON STREET

•152 •158 •202 •206 •212

•1192-23 •11909-11 •11811 •11801-03

VAC

W. FARMERS AVE.

SOUTH 118TH STREET

•11725

•11815-17-19

•150

•11721 •11735 •11739 •11811

•11624 •11629 •11700 •11712

WEST DIXON

•1169-21 •11629 •11701 •11701

